



MILLENNIUM CHALLENGE ACCOUNT NEPAL (MCA-NEPAL)

Minutes of Pre-Offer Conference

**“Recycling Pilot Works Contract – Dhan Khola Lamahi 40km
Road Section”**

Ref No: (MCA-N/RMP/CB/008)

VENUE: YAK & YETI HOTEL, CRYSTAL HALL, KATHMANDU

15 November 2024

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I. MEETING

The Pre-Offer Conference session was called to order by the Procurement Agent Manager of the MCA-Nepal Procurement Agent who welcomed all the attendees and briefly highlighted the conference agenda and its objectives. The Pre-Offer Conference started from 10:00 AM (Nepal Standard Time) on 15 November 2024.

II. AGENDA

- Welcome remarks by the Procurement Agent
- Welcome Remarks by MCA-Nepal
- Presentation on technical aspects of Bidding Document
- Question and Answers -Extended Session
- Lunch Break
- Procurement Briefing on the Bidding Document
- Question and Answers
- Closing Remarks

The agenda is attached in Annex A.

III. MEMBERS' PRESENT

The attendance sheet is attached in Annex B.

IV. Welcome by Procurement Agent

The Procurement Agent Manager of the Procurement Agent of MCA-Nepal welcomed all the conference participants and provided a brief presentation about the objectives of the Pre-Offer Conference and agenda. He also highlighted the following points:

- (i) In case of any discrepancy between the presentations and the Bidding Documents or its Addenda, the Bidding Document and issued Addenda will prevail.
- (ii) The responses provided during this meeting are only draft answers. The minutes of the meeting will contain the responses in writing and that will be final.
- (iii) The prospective offerors are requested to submit all the queries/questions in writing.

V. Welcome Remarks by MCA-Nepal

The Executive Director of MCA-Nepal welcomed all participants, highlighted the importance of this procurement for MCA-Nepal related to the Road Management Project of MCA-Nepal. He also requested/encouraged to all the prospective offerors to ask all their queries in writing by the clarification deadline and MCA-Nepal will take the note of all questions and provide the answers.

VI. Presentation on technical aspects of Bidding Document

Prabin Thapa, Road Maintenance Specialist at MCA Nepal presented the technical information regarding Full Depth Reclamation (FDR) Pavement design and Superpave Asphalt Implementation for the East – West Highway – Dhan Khola Lamahi Road Section (DLRS). He further highlighted the scope of the project, specific construction measures,

and technologies to be used, road design details and construction and implementation schedule.

Mr. Prakash Basnet, ESP Specialist highlighted about the Government of Nepal Environmental Regulations and MCC Environment Guidelines. He further highlighted the key aspects of the Initial Environmental Examination (IEE) for the Environmental for Dhan Khola Lamahi Road Section (DLRS). In addition to that, he also discussed the Environmental, Social, Health and Safety Plan (ESHSM) for DLRS and highlighted the need for the Contractors to prepare Contractor's Environmental, Social, Health and Safety Plan (CESHSMP) prior to the start of works.

Ms. Kalawati Rai – GSI specialist highlighted the key aspects of the Gender and Social Inclusion (GSI) requirements of the project. The presentation focused on Prevention of Gender based Violence, Sexual Harassment, Child Employment, Prevention and Mitigation of Trafficking in Persons (TIP) Risk, Labor and Employment Opportunities including Women Employment and Staffing throughout the Project Period.

VII. Procurement Briefing on the Bidding Document (Procurement Presentation)

The second session was presented by Mr. Rakesh Kumar, Senior Procurement Specialist from the Procurement Agent of MCA-Nepal. The session highlighted the Bidding Document, emphasizing important provisions of the Bidding Document to avoid common mistakes in the Offer submission and where Offerors need to have a clear understanding to develop a sound Offer such as clarification deadline, submission deadline, procedure to submit/upload the Offer, qualification evaluation criteria and submission forms.

VIII. Question and Answers– Pre-Offer Conference

After the presentations, the participants of the bidders' representatives were provided with the opportunity to ask queries/questions. The queries raised by the participants were responded by MCA-Nepal team comprising of Technical, Procurement and Procurement Agent team. The questions raised by the offerors' representatives and the responses provided by the MCA-Nepal team are presented in Annex D.

IX. End and Closing Remarks

The Deputy Executive Director - Management of MCA-Nepal thanked all the participants for attending/joining the conference and for their active participations and requested to prepare the proper offer taking consideration to include the Environmental and Social (E&S) Management, Health and Safety (H&S) Management Experience, if they have such experiences. She also encouraged to request any clarification the prospective offerors might have so that MCA-Nepal will try its best to provide the answer as soon as possible.

The Pre-Offer Conference was closed at 2:42 PM (Nepal time).

Annexes:

Annex A: Agenda

Annex B: Attendance Register

Annex C: Presentations

Annex D: Questions and Answer

Annexes/Attachments

Annex A



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Offer_RMP_FDR.pdf



MILLENNIUM CHALLENGE ACCOUNT NEPAL (MCA-NEPAL)

Pre-Offer Conference - “Recycling Pilot Works Contract – Dhan Khola Lamahi 40km Road Section (MCA-N/RMP/CB/008)”

15 November 2024 starting from 10.00AM (Nepal Time)

Venue: Hotel Yak and Yeti- Crystal Hall, Durbar Marg Kathmandu, Nepal, and through Zoom web conference

AGENDA

15 November 2024

10:00 AM to 10:30 AM	• Registration
10:30 AM to 10:35 AM	• Welcome by Procurement Agent
10:35 AM to 10:45 AM	• Welcome Remarks by MCA-Nepal
10:45 AM to 11:45 AM	• Presentation on technical aspects of Bidding Document
11:45 AM – 12:00 PM	• Question and Answers -Technical Aspects
12:00 PM – 1:00 PM	• Lunch
1:00 PM – 1:40 PM	• Procurement Briefing on Bidding Document
1:40 PM – 2:00 PM	• Question and Answers – Procurement Aspects
2:00 PM - 2:15 PM	• Closing Remarks

The Government of Nepal established MCA-Nepal on 18 April 2018 under Development Board Act, 2013 BS as the accountable entity to implement a program co-funded by Nepal and US Government’s Millennium Challenge Corporation.

Lal Durbar Convention Centre, Yak & Yeti Complex, Durbar Marg, Kathmandu
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Roads and Electricity, Way to Prosperity

Annex B

Attendance register – Pre-Offer Conference



Attendance-External.
pdf



Millennium Challenge Account Nepal (MCA-Nepal)
REGISTRATION SHEET

Program: Pre-Offer Conference for Procurement of Recycling Pilot Works Contract – Dhan Khola Lamahi 40km Road Section

Date: 15 November, 2024

Venue: Crystal Hall, Hotel Yak and Yeti

S.N.	Name of Organization/Company	Name(s) of Representative	Designation	Email	Country	Signature
1	Amar Construction Pvt.Ltd	Jebin Bikram Shahi	Managing Director	Shahi.jebin@gmail.com	Nepal	
2	C.A.B Construction Pvt. Ltd.	Basant Bahadur Chand	MD	basantchand69@gmail.com		
3	C.A.B Construction Pvt. Ltd.	Dhiraj Chand	Manager	dhirajchand05@gmail.com		
4	C.A.B Construction Pvt. Ltd.	Hemant Chand	Director	chandhemant02@gmail.com		
5	Hanuman Construction Pvt. Ltd.	Manish Sharma	Procurement Head	hcprojects.nepal@gmail.com	Nepal	
6	Hanuman Construction Pvt. Ltd.	Mr. Trishna Pokharel	Site Engineer	hcprojects.nepal@gmail.com	Nepal	
7	Kankai International Builders Pvt. Ltd.	Hari Bhakta Pokharel	Project Manager	scpS34@kankaibuilders.com	Nepal	
8	Kankai International Builders Pvt. Ltd.	Sushant Mainaly	Director	sushant.mainaly@kankaibuilders.com	Nepal	
9	M/S NIRBHAY CONSTRUCTION	Nirbhay Pratap Singh	PROPRIETOR	nirbhayconstructions@gmail.com		



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Date: 15 November, 2024

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S.N.	Name of Organization/Company	Name(s) of Representative	Designation	Email	Country	Signature
10	Mahabir Engineering and Construction Pvt. Ltd.	Sumit Kumar Thakur	Engineer	mahabireng@gmail.com	Nepal	
11	Mahabir Engineering and Construction Pvt. Ltd.	Anil Gupta	Managing Director	mahabireng@gmail.com		
12	Markolines	Intzar Ali	GM - Highway Maintenance	intzar.ali@markolines.com	India	
13	Markolines	Luv Yadav	Quality Control Engineer	luv.yadav@markolines.com	India	
14	RAMAN CONSTRUCTION	Dipak Chhetri	Senior Admin Officer	tender@ramanconstruction.com	Nepal	
15	RAMAN CONSTRUCTION	Pawan Mahato	Managing Director	pmahato@ramanconstruction.com	Nepal	
16	Rautaha Construction P. Ltd.	Henif Dahal	Engineer	rautahac@gmail.com		
17	Rautaha Construction P. Ltd.	Shishir Kondongwa	Engineer	rautahac@gmail.com	Nepal	
18	Samanantar Nirman Sewa Pvt. Ltd.	Mahesh Bhatta	Project Director	mahesh@samanantargroup.com	Nepal	



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S.N.	Name of Organization/Company	Name(s) of Representative	Designation	Email	Country	Signature
19	Samanantar Nirman Sewa Pvt. Ltd.	Rakesh Shadha	Project Manager	rakeshshadha12@gmail.com		
20	Samanantar Nirman Sewa Pvt. Ltd.	Sailesh Pathak	Procurement Manager	sailesh@samanantargroup.com	Nepal	
21	Swachchhanda Nirman Sewa (P) Ltd.	Sujit Shrestha	PM	sujit@snsLtd.com.np	Nepal	
22	Swachchhanda Nirman Sewa (P) Ltd.	Subhakar Bhusal	PM	business@snsLtd.com.np		
23	Tundi Construction Pvt. Ltd.	Kashi Nath Dotel	Chief Technical Officer	kn.dotel@tundigroup.com	Nepal	
24	Tundi Construction Pvt. Ltd.	Saroj Subedi	Business Development M	saroj.subedi@tundigroup.com	Nepal	
25	Vedalya Construction Pvt.Ltd	Rakesh Bhattarai	Civil Engineer	rakeshbhattarai111@gmail.com		
26	Vedanshee Infrastructure Pvt. Ltd	Nirmal Panta	Engineer	mithonirmal@gmail.com	Nepal	
27	Vedanshee Infrastructure Pvt. Ltd	Nicholas Pandey	Managing Director	pandeynicholas@gmail.com	-	-



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S.N.	Name of Organization/Company	Name(s) of Representative	Designation	Email	Country	Signature
28	Vedanshee Infrastructure Pvt. Ltd	Prakash Chandra Kharel	General manager	pc_kharel@yahoo.co.in	—	—
29	Alex Apeayye	MCA - N		a.a.peayye@gmail.com	UK	
30	Prameshwar Rai Adhikari	Contech P. Ltd	Project Coordinator	Prameshwarcontech@gmail.com	Nepal	
31	Lana Construction Co. P. Ltd	Ashok Tripathi	Tender	lanaconstruction.nepal@gmail.com Nepal		
32	Bidasi Construction Pvt. Ltd	Kedon Pragy	Director	publichpvt@gmail.com	Nepal	
33	Rohit S Aghave	SWECO/NECO	Project coordinator	rohitsaagave	Nepal	
34	Bidasi Const Pvt Surenchandra Rai	Surenchandra Rai	MD Director	Kirnuwal.nepal.com	Nepal	
35	Ashish Nirman Gurli	Jyoti Subedi	Expert	ashishnirmansewa@gmail.com	Nepal	
36	ANK Construction Co. Pvt. Ltd	Kabin Maharjan	Dir	Kabin@ank.com.np	Nepal	



Millennium Challenge Account Nepal (MCA-Nepal)

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S.N.	Name of Organization/Company	Name(s) of Representative	Designation	Email	Country	Signature
37	MCC	Leshy Karki	Comms Specialist			
38	Tundi Construction	Adwitiya Subedi	Engineer		Nepal	
39	P.S. Construction	Ang Dangi	Engineer		Nepal	
40	Bandan Bhagwati Mirman Sewa	Arpan Adhikari	Engineer		Nepal	
41	Shyamsundar Mirman Sewa	Binay Kr. San	Engineer		Nepal	
42	Khadka Krishna Construction Pvt. Ltd	Prakash Khadka	M.D	khadka@kfishhu2070@gmail.com	Nepal	
43	Dewa Nirman Sewa Pvt. Ltd	Rajish Adhikari	M.D	Diwonirman970@gmail.com	Nepal	
44	Tundi Construction Pvt Ltd	Aashma Karki	Admin		Nepal	



Millennium Challenge Account Nepal (MCA-Nepal)

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S.N.	Name of Organization/Company	Name(s) of Representative	Designation	Email	Country	Signature
45	GCE Group	Somrat Vaidya	BDM	somrat.baidya@gce.com.np	Nepal	
46	Bansiya N.S.	Vijay Bhattarai	Chief Ex.	vijaybhattarai@gmail.com	Nepal	
47	Sagun construction	Sandeep Khadka	Director	sandeepkhadka73@gmail.com	Nepal	
48						
49						
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51						
52						
53						
54						

Attendance from Zoom

Registrants for 'Pre-Offer Conference - Recycling Pilot Works Contract– Dhan Khola Lamahi 40 km Road Section'	
S.N.	Registrants
1	Luis Villalta
2	Surendra Pandey
3	Bikram Gautam
4	Pushparaj Khadka
5	Siddharth Gupta
6	Shreyansh Tiwari
7	Rajkumar Bidari
8	Vaibhav Pandey
9	Jay Chauhan
10	Dambar Pal
11	Arjun Khatiwada
12	Rupak Lamichhane
13	Arpan Adhikari
14	Dipesh Rimal
15	Gabriel J. Assaf
16	Mahendra Lamsal
17	Manish Regmi
18	Ashok Kumar Garg
19	Binita Poudel
20	Nabina Mahar
21	Partho Biswas
22	Nirbhay Pratap Singh
23	Siddharth Gupta
24	Rakesh Sharma
25	Sushil Garg
26	Geoffrey Rowe
27	Gaurav Kumar
28	Krishna Gupta
29	Saurabh Kushwaha
30	Vairavan S
31	Saroj Subedi
32	Ganesh Adhikari
33	Junu Magar
34	Ganesh Adhikari
35	Vivek Kumar Sinha_Blacklead
36	Intzar Ali
37	Aniket Singh
38	Himanshu Vashist
39	Shree Dangol
40	Rahul Garg
41	Sushil Kumar Shrestha
42	Shivam Agrawal
43	Anurag Chaturvedi

Registrants for 'Pre-Offer Conference - Recycling Pilot Works Contract– Dhan Khola Lamahi 40 km Road Section'	
44	Jay Nishaant
45	Sitaram Shrestha
46	Anil Sharma
47	Suhail Alvi
48	Uddhav Acharya
49	Tapan Parikh
50	Dr JB Singh Markolines
51	Sushant Mainaly
52	Bibhudutta Satpathy
53	Sukhmani Kaur
54	Dhyey Hirpara
55	Prashant Yadav
56	Shriniwas Nagarkar
57	Saurav Sharma
58	Pawan Joshi
59	Suroj Ghimire
60	Pooja Pawar
61	Dev Tamang

Annex C

Presentations



04_241115 Pre-Bid Conference Pilot Worl



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02_241114_RMP_GSI _KR.pptx



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Road Maintenance Project

Recycling Pilot Works– Dhan Khola Lamahi 40 km Road
Section

Pre-Offer Conference

15 November 2024



Objectives of the Pre-Bid Conference



Objective of the Pre-Offer Conference

- Provide Relevant Information/Clarifications To Prospective Offerors
 - Technical Information
 - Environmental, Social, GSI
 - Procurement Information
- Discuss Important Highlights of the ITO
 - Provide Tips and recommendations for submission of Offer
- Receive Questions Clarifications from Prospective Offerors





Agenda

- From 10:00 AM to 2:30 PM
- During first Half: Presentations on technical aspects
- Questions and Answers Session
- Lunch Break
- During Second Half : Presentation on Procurement
- Questions and Answers Session
- Closing Remarks



Timeline



Event	Date	Remarks
Publication Date	30-Oct-24	
Site visit	13-Nov-24	Completed
Pre-Bid conference	15-Nov-24	Today
Last date for requesting Clarification	21-Nov-24	25 days prior to the bid submission deadline
Last date for responding to request for Clarification	28-Nov-24	18 days prior to the bid submission deadline
Submission Deadline	16-Dec-24	
Offer Validity	13-Aug-25	240 days
Validity of Bid Security	10-Sep-25	



Question and Answers

- MCA-Nepal will try to provide best answers to raised queries
- All answers shall be considered as “draft answer” as the official shall be in writing
- It is possible that some clarifications and its responses may trigger an addendum to the bidding document.
- Minutes of the meeting and recording of the meeting (as available) will be circulated



Roads and Electricity, Way to Prosperity.



Road Maintenance Project (RMP)

RMP Pilot Works Pre-Offer Conference

15 November 2024



ESP-Requirements

- MCA-Nepal/MCC, ESP compliance is essential for the project;
- Government of Nepal Environmental Regulations (EPR 2020) – an IEE is required;
- MCC Environmental Guidelines – Category B project. An ESIA is required;
- Must align with MCC Environmental Guidelines and IFC Performance Standards (PS 1-8);
- All risks/impacts related to construction must be avoided, mitigated, or compensated.

- Key ESP Documents:
 - IEE/ ESIA, ESHSMP/EMP and RAP document;
 - These documents identify construction-related impacts, mitigation measures, and associated costs.



ESP-Requirements contd..

➤ **Contractor's Responsibilities:**

- Contractors must establish a qualified ESP team to meet the Employer's standards;
- Contractors must adhere to ESHSMP practices and develop their own Contractor's Social, Health, and Safety Management Plan (CSHSMP);
- CSHSMP requires approval 30 days before field mobilization;
- Implementation of the CSHSMP should ensure that environmental safeguards effectively mitigate adverse impacts of the transmission line project.

➤ **Key Components of CESHSMP**

- Occupational Health and Safety Plan;
- Community Health and Safety Plan;
- Emergency Preparedness and Response Plan;
- Traffic Management Plan;
- Grievance Redress Mechanism (GRM);
- COVID-19 Response Plan;
- Worker's Code of Conduct;
- Copies of all required permits;
- Workforce Management Plan.



ESP-Requirements contd..

Key Environmental and Social Risks and Impacts:

➤ Infrastructure Affected

- 406 electric poles, 47 transformers, 2 water supply schemes, 1 tap stand, 2 welcome gates, 17 passenger sheds, temporary APF check post, and 1 Irrigation Canal.

➤ Physical Environment Impacts

- Landslide and soil erosion (various sections)
- Noise pollution in construction sites, market areas, schools, and hospitals;
- Air and water pollution due to equipment operation and waste discharge in rivers;
- Waste generation (solid, hazardous, and construction) from camps and plants.

➤ Biological Environment Impacts

- Loss of 195 trees;
- Disturbance to wildlife movement and exploitation of forest resources.



ESP-Requirements contd..

➤ **Socio-Economic & Cultural Impacts:**

- Loss of banana and bamboo plants affecting households;
- Pressure on public utilities from worker influx;
- Safety risks for workers and the public, risk of infectious diseases;
- Increased risk of GBV due to temporary worker influx;
- Ribbon settlements, ROW encroachment, and road accidents in specific areas.

➤ **Positive impacts:**

- **Employment Generation:** Creates approx. 75,000 skilled and 130,000 unskilled person-days of employment;
- **Economic Growth:** Boosts local economy through job opportunities, increased market activities, and potential business growth along the road alignment;
- **Improved Accessibility:** Enhances access to social services, economic centers, and markets, reducing travel time, fuel consumption, and vehicle emissions;
- **Operational Benefits:** A widened road with an improved surface reduces dust, lowers road maintenance and vehicle operational costs, and provides faster, safer travel.



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Road Maintenance Project (RMP)

RMP Pilot Works Pre-Offer Conference

15 November 2024



GSI-REQUIREMENTS

1. Prevention of Gender-based Violence, Sexual Harassment, Child Labor:
 - Zero tolerance policy against sexual harassment
 - Develop and implement an Anti-Sexual Harassment Policy before starting work

2. Prevention and Mitigation of Trafficking in Persons (TIP) Risk:
 - Zero tolerance policy against TIP
 - RMP project sites are at high risk of TIP
 - Develop and implement a TIP Risk Management Plan



GSI-REQUIREMENTS

3. Labor and Employment Opportunities including women employment:
 - Work Plan, Methodology to prioritize women workers
 - Safe and harassment-free working environment
 - Anonymous Complaint Mechanism System

4. Staffing throughout the Project Period:
 - Dedicated GSI Officer
 - Close coordination with the ESP and H&S team
 - Regular training, communication and reporting

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Road Maintenance Project (RMP)

RMP Pilot Works Pre-Offer Conference

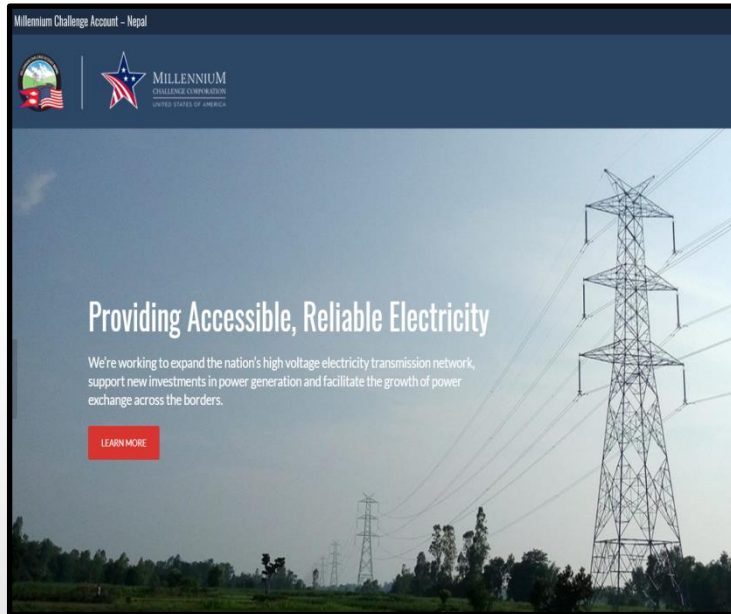
15 November 2024



Overview of MCA-Nepal Compact

MCA-Nepal Compact consists of Two Projects

Electricity Transmission Project (ETP)



- 315km of 400kv electric transmission lines
- Three substations
- Technical Assistance (TA) to Nepal Electricity Authority and Electricity Regulatory Committee



- A \$500M Compact, with a \$197M government contribution, for a combined program total of \$697M
- RMP \$52M

Road Maintenance Project (RMP)

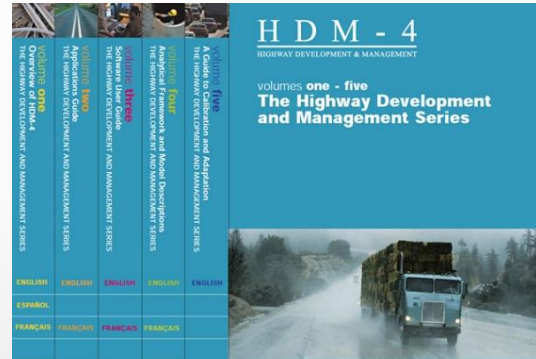


- Up to 90km of Enhanced Road Maintenance
- Technical Assistance (TA) to Department of Roads and Roads Board Nepal

Road Maintenance Project (RMP)

➤ Project components

Technical Assistance



- Pavement Preservation and Rehabilitation Manual
- Improved road maintenance system
- Increased funding for road maintenance
- Integration of New Pavement Technologies in Nepal

Matching Funds for Maintenance Works



- Sub-Activity 01 new pavement recycling technologies introduction FDR and Superpave
- Sub-Activity 02 periodic road maintenance with the introduction of Asphalt milling technology



Road Maintenance Project (RMP)

➤ New Technology Introduction

FDR + Resurfacing (Superpave)

When there is a need for additional structural strength
Reduces deterioration rate and therefore transportation cost

DHAN KHOLA LAMAHI ROAD

Milling + Resurfacing (Superpave)

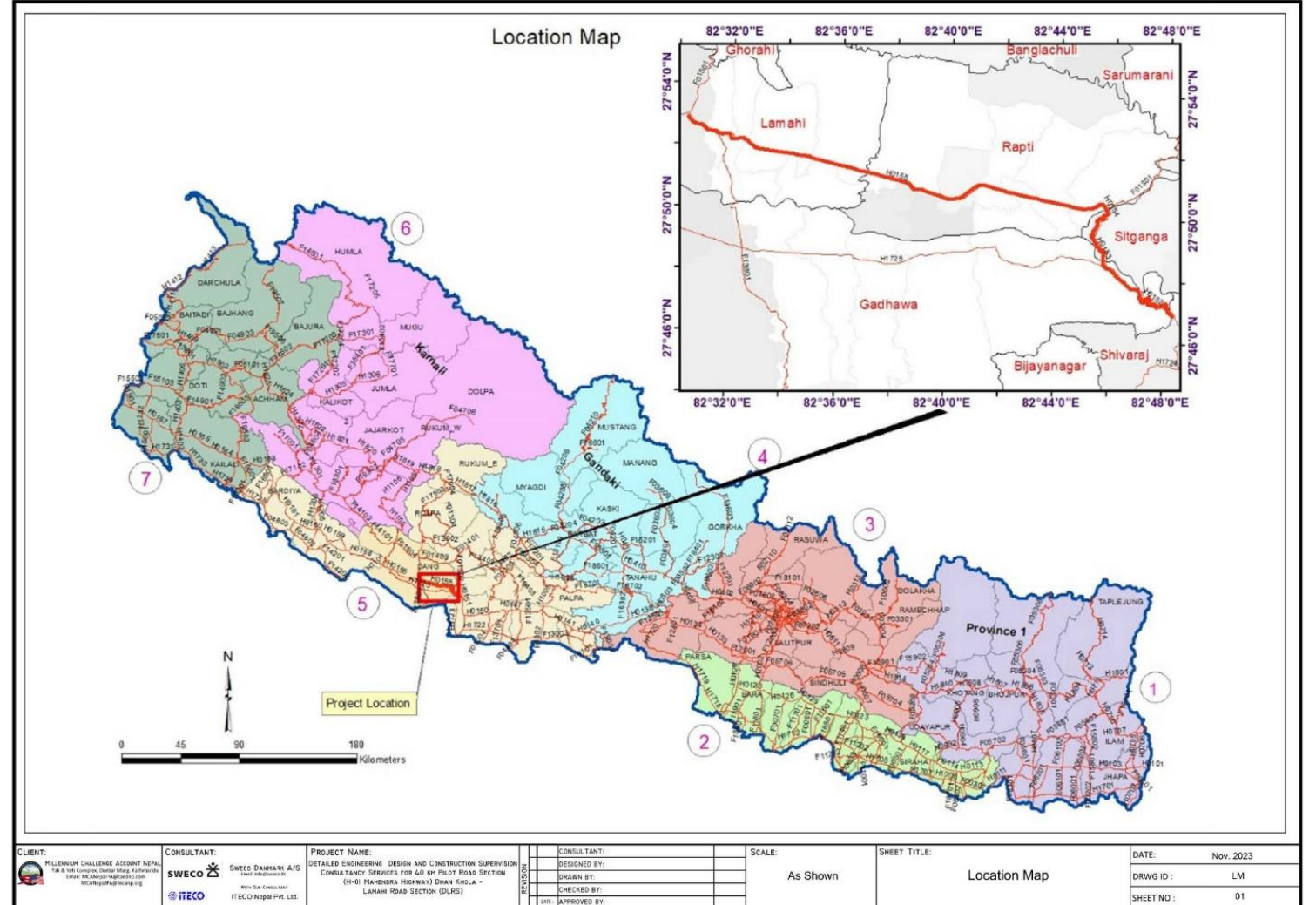
When there is only a need to improve driving comfort
Reduces transportation cost

NARAYANGHAT MUGLING ROAD



Project Definition/Scope

- Dhan Khola Lamahi Road Section (DLRS) lies in Dang and Arghakhanchi districts of Lumbini Province
- Project road starts from Dhan Khola Bridge and ends at Arjun Khola near Lamahi Junction (way to Ghorahi)
- Project starting point is 345 km from Kathmandu, and 110 km from Sunwal border with India





Project Definition/Scope

- RMP Objective: Maintain road quality of SRN by using new technology for pavement strengthening such as FDR and Superpave in Nepal including improved traffic safety
- Dhan Khola - Lamahi Road Section (DLRS) constructed in 1985, resurfacing (Double Bituminous Surface Treatment (DBST)/Bituminous carpeting) has been conducted thereafter
- International Road Assessment Program (iRAP) recommendation: Countermeasures to improve safety “Star” rating to at least a 3-Star or higher
- Recommended road width: International Road Assessment Program (iRAP) assessment and DOR geometric standard for Highway- 7m carriageway with 2.5m shoulders
- Improves roughness (Key performance indicator): The target International Roughness Index (IRI) is 1.2 to 1.7 m/km



Key Elements of Project

- **Roughness**

International Roughness Index (IRI) measured on outer wheel path from Dhan Khola to Lamahi 8.75 m/km and Lamahi to Dhan Khola 8.89 m/km

Target IRI is 1.2 to 1.7 m/km

- **Distress**

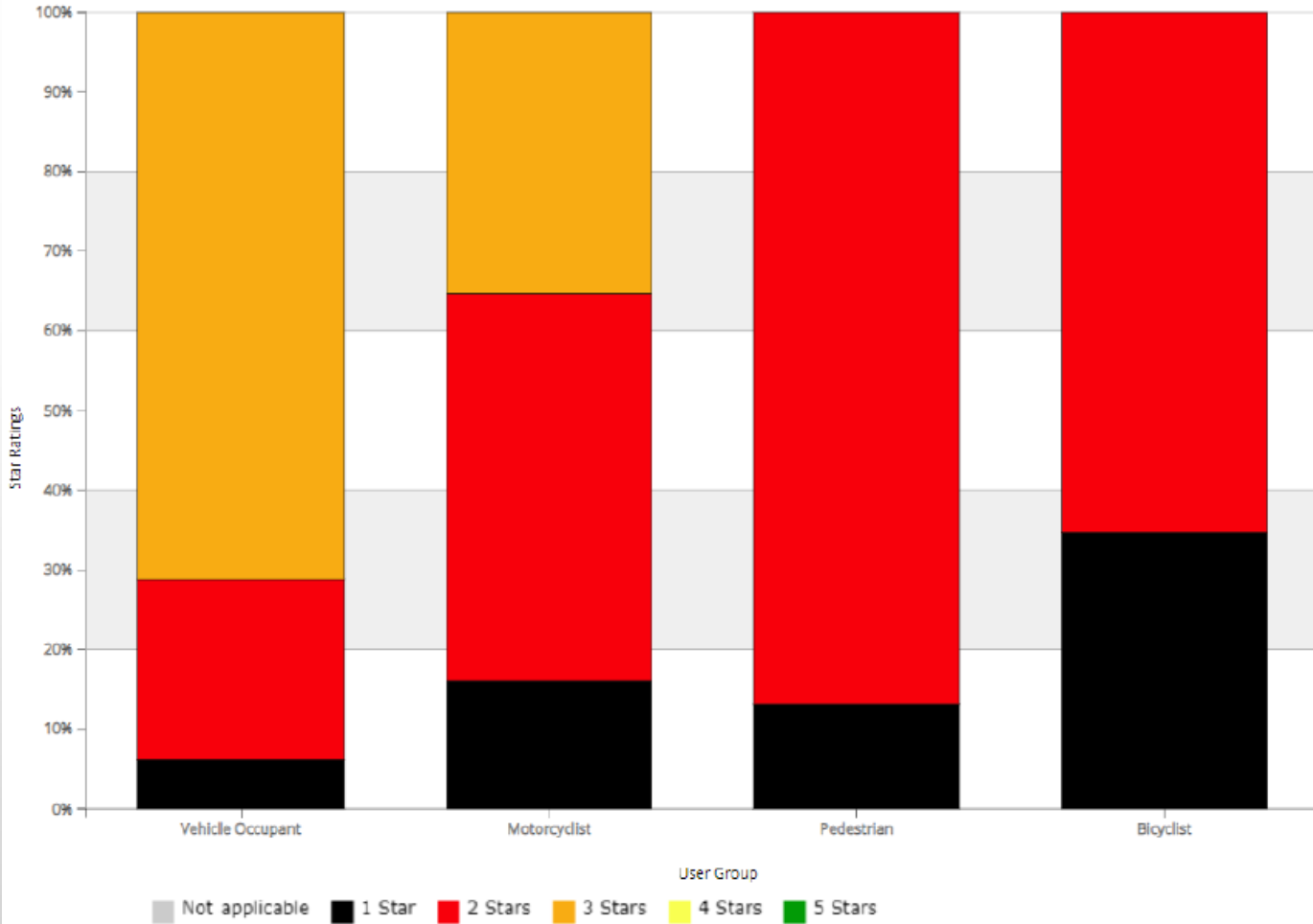
The main distresses observed are alligator cracking, disintegration and potholes

Edge Cracking	Alligator Cracking	Disintegration	Potholes	Patching	Bleeding
2.5 per km	25%	14.9 per km	10.6 per km	16.8 per km	4%



Key Elements of Project

- iRAP Assessment of DLRS**



Dhankhola - Lamahi road is currently rated at **1, 2 and 3 stars**

Target **iRAP** countermeasure is to upgrade the road to **3 stars**

Average Annual Daily Traffic (AADT) was 5,436 vehicles per day in 2022 according to the DoR



Key Elements of Project

iRAP Countermeasures Adapted

- Widening to be limited to two locations only at Bhalubang and Lamahi Bazar
- Improve minor intersections
- Improve all the major intersections
- Provide kerb-stones for segregated sidewalks along the ribbon development of the Road
- Provide curvature improvements
- Provide WCLT along the Dhan Khola- Bhalubang section and five semi-urban areas

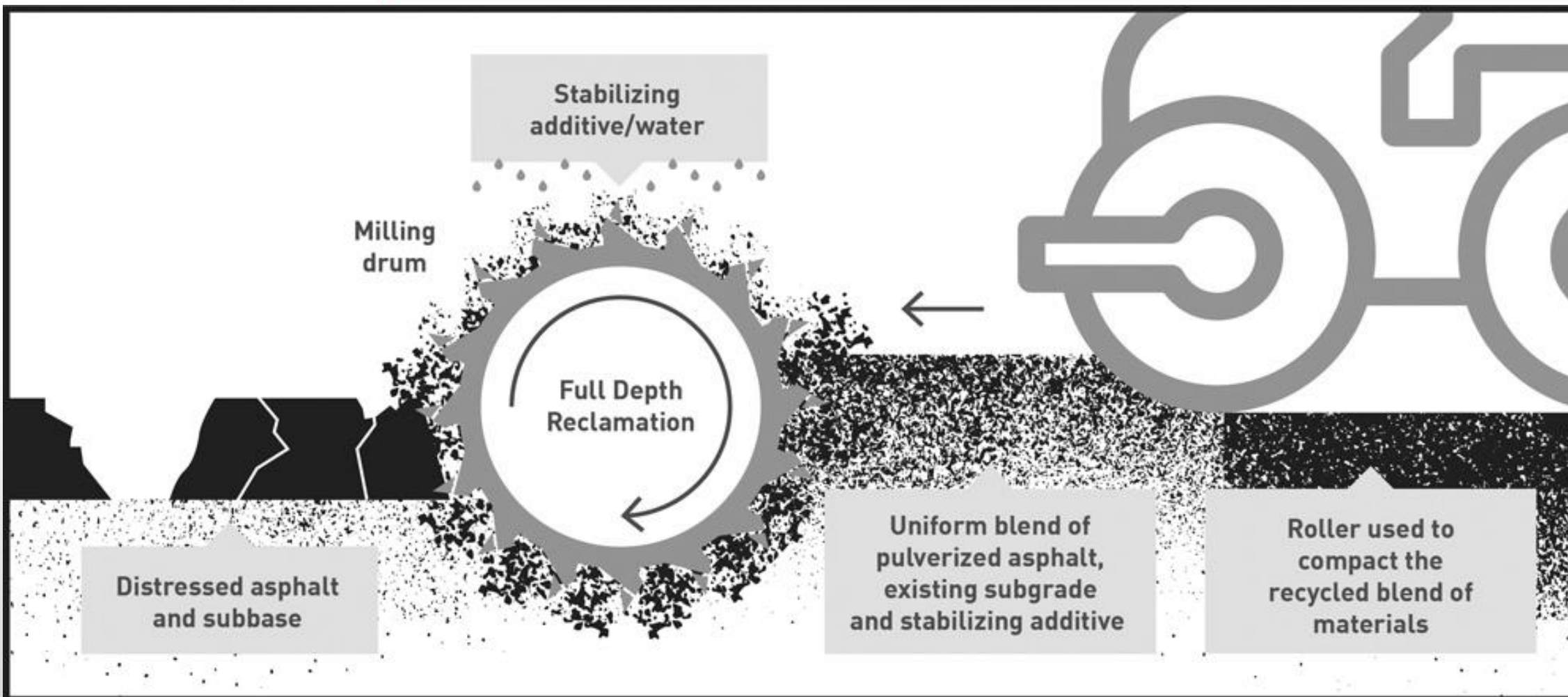


Project Implementation Arrangement

- MCA-Nepal: as the implementation agency, tasked with implementing and monitoring both civil works and environmental and social mitigation measures
- MCC: will provide oversight and support to MCA-Nepal throughout the project's implementation
- Department of Roads (DoR): will oversee the design and implementation of the works and will ultimately take charge of the road upon its completion. Further DoR will continue FDR projects because of cost effectiveness and durability
- Supervising Consultant Engineer: will supervise the works and the environmental and social mitigation measures; they report to MCA-Nepal
- Contractor: performs all construction activities, including environmental and social safeguards
- Once the Defect Notification Period (DNP) is concluded, the road section will be transferred to the Department of Roads



Specific Construction Measures or Technologies



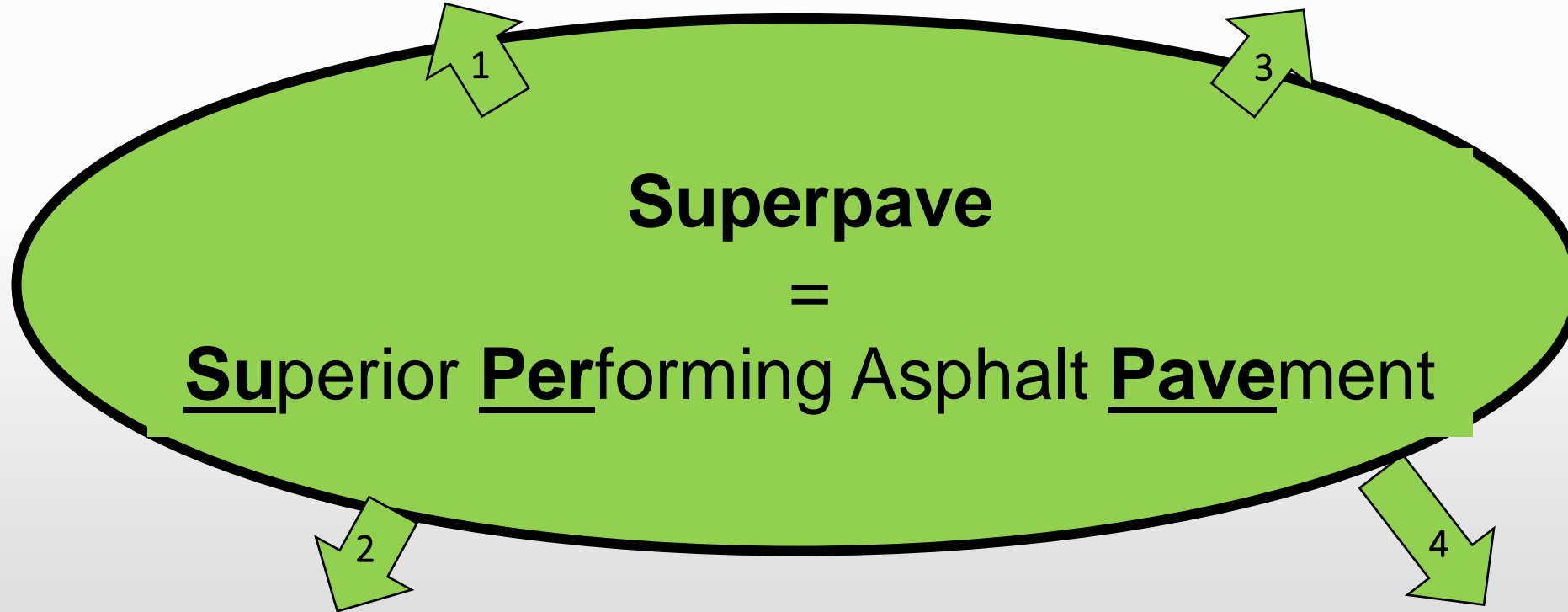
Full Depth Reclamation Process



Specific Construction Measures or Technologies

Simulates ageing in the laboratory

Simulates real compaction efforts to assess compactibility



Accounts for traffic loading and environmental conditions

Accounts for rutting and cracking

Superpave Process
(replaces Marshall Method to improve pavement performance over life cycle)



Specific Construction Measures or Technologies

- **First time in Nepal: Full Depth Reclamation (FDR), an innovative and cost-effective road construction technology.**
 - involves recycling existing pavement materials;
 - process involves applying and uniformly mixing Portland or a mix of cement and water, followed by compaction using padfoot, smooth drum, and/or pneumatic-tired rollers
 - followed by shaping with motor graders, and ensuring proper curing until full strength
 - The depth of pulverization is 250mm and 300mm.
 - followed by the application of two layers of Superpave asphalts.
- **First time in Nepal: Superpave**
 - Longer-lasting asphalt pavement compared to normal asphalt concrete
 - Better consideration of climate, -22° to $+76^{\circ}$ C (Bitumen Grade: PG76H-22)
 - Customizable as per the site





Specific Construction Measures or Technologies

FDR and Superpave - Why these technologies

➤ FDR

- ✓ Recycles and reuse
- ✓ Makes use of local binder – cement
 - Avoids import and use of Nepal product
- ✓ Produces a stronger base
- ✓ Base not sensitive to moisture fluctuations
- ✓ Can stabilize soils as well as granular materials



➤ Superpave

- ✓ Longer lasting asphalt pavements
- ✓ Further develops viscosity based specifications
- ✓ Better consideration of climate
- ✓ Balanced mix design concepts and on site performance testing



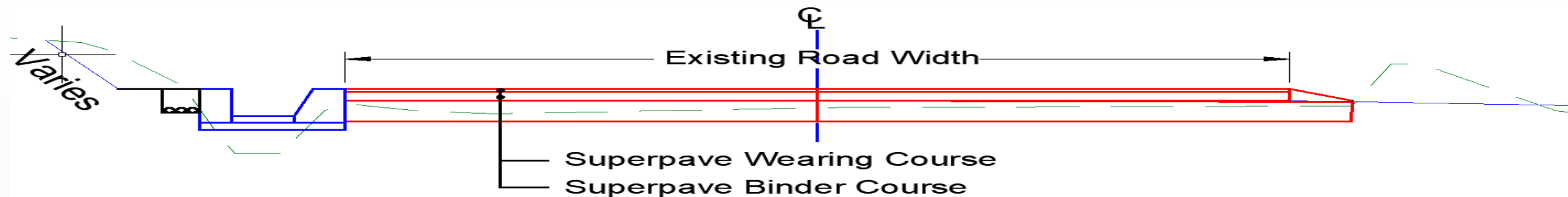


Road Design Details

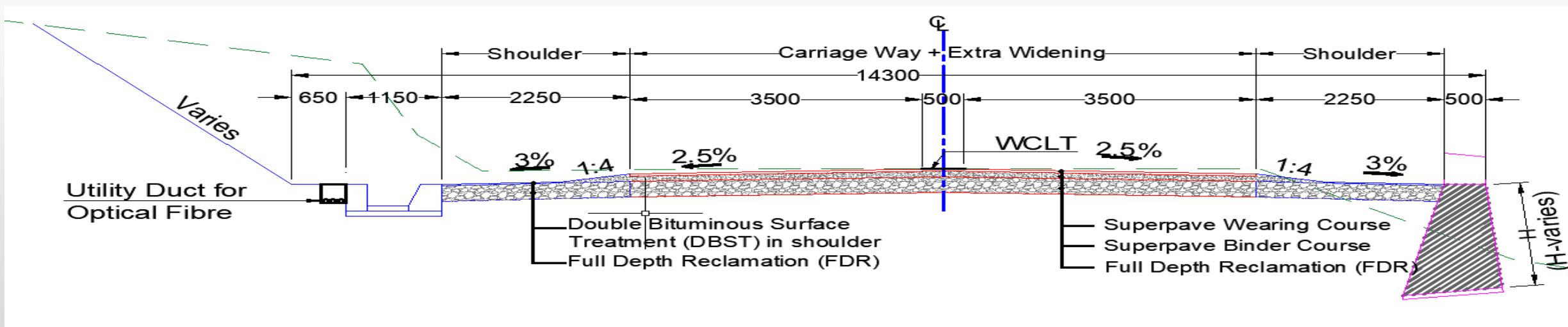
- Carriageway of 7m and shoulder width of 2.5m. In the case of the Wide Centre Line Treatment (WCLT) section, the Shoulder width will be 2.25m
- Lane separation by providing WCLT of 0.5m to reduce head-on collision **at hilly terrain** and **5 semi-urban** areas namely Pakhapani, Pipari, Sishaniya, Narti and Bankatti
- Avoidance of extensive cutting of fragile slopes as recommended by geotechnical expert/geologist
- 2-urban areas (Bhalubang and Lamahi) proposed for service lanes with medians
- Improvement of 2 major junctions at Bhaluwang and Arjun Khola. In Arjun Khola, roundabout is proposed
- Access Road Improvement: length of 15m with DBST
- Drainage improvement by means of extension, replacement and new culverts and side drains
- Improvement of road safety, road markings, barriers, rumble strips and signage
- 650mm wide space at the hillside for Utility Duct for Optical Fiber Cables



Road Design Details



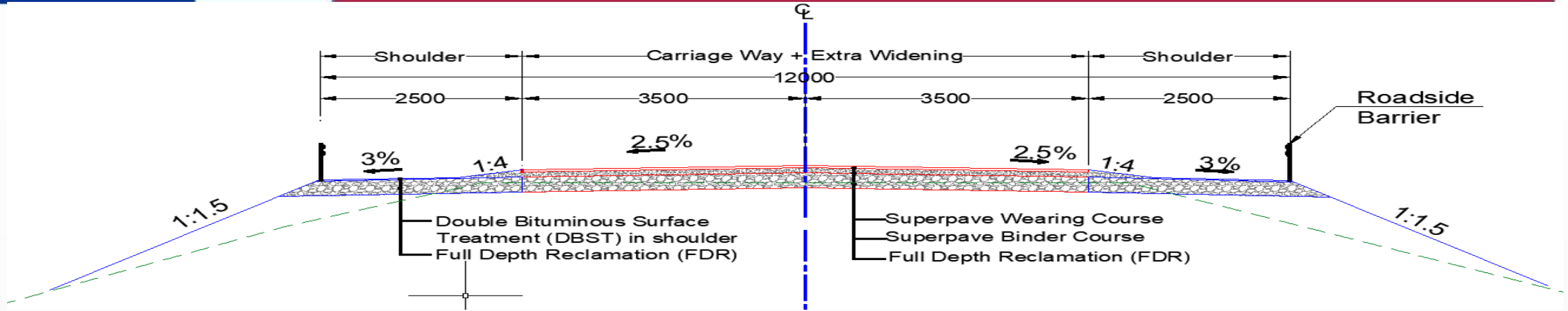
Typical X-section at Dhan Khola Bazaar Area (720m)
(From Km 676+000 to Km 676+720)



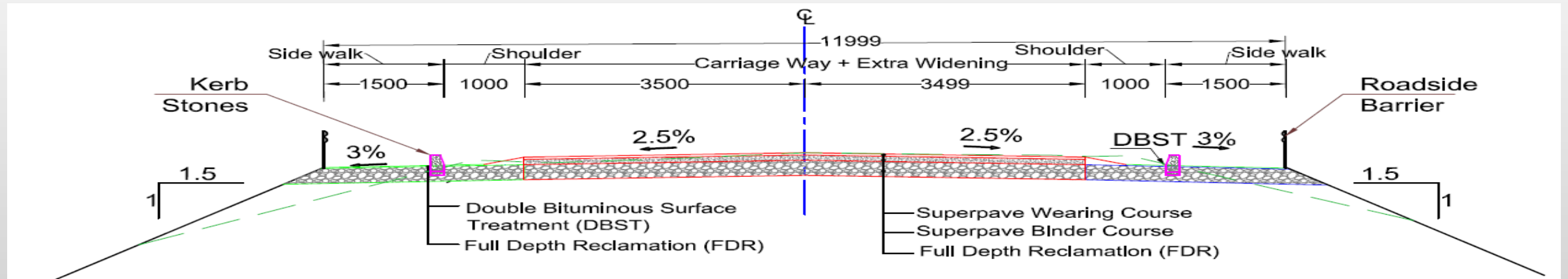
Typical X-section for hilly section with WCLT
(From Km 676+720 to Km 688+067)



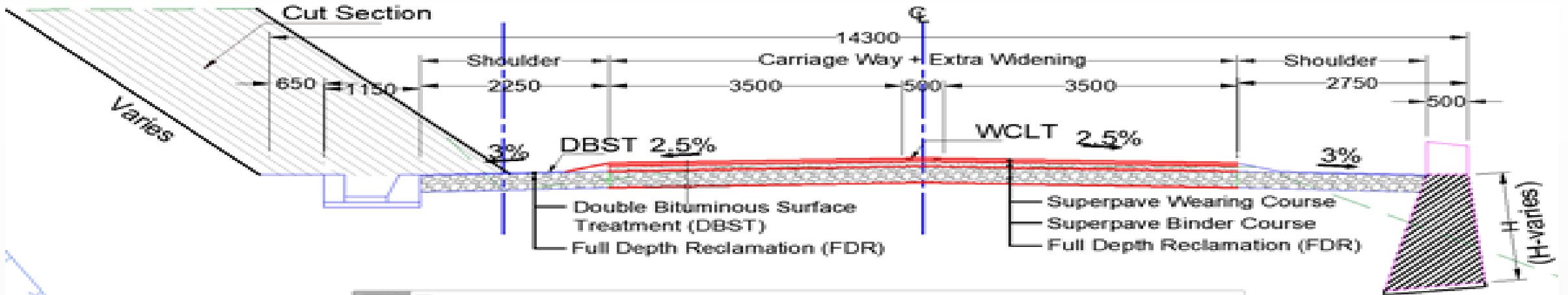
Road Design Details



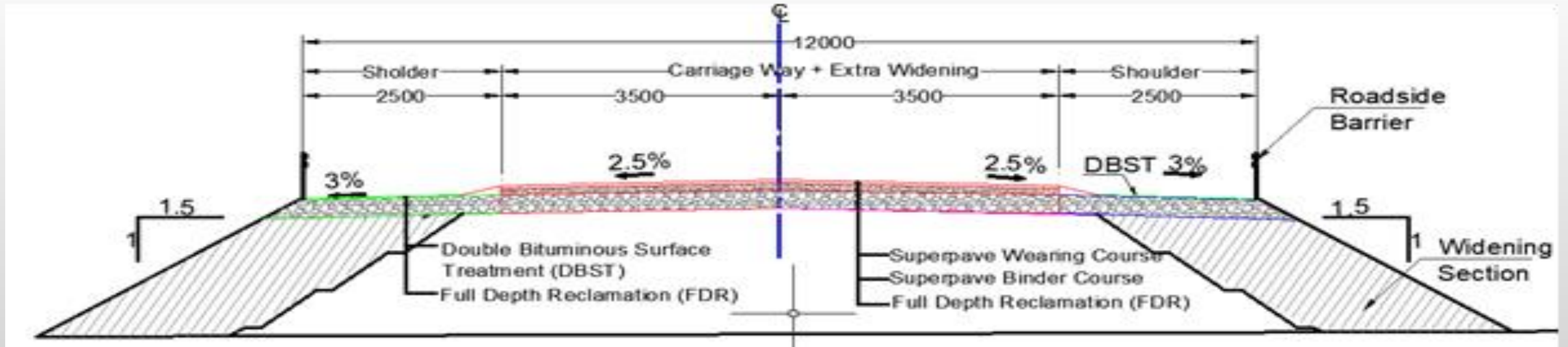
Typical X-section of plain/flat rural section- Bhalubang to Lamahi
(From Km 688+067 to Km 714+985 -)



Typical Section showing Provision of Sidewalk



Typical Section: Hilly Section with widening



Typical Section: Plain Rural Section with widening



Proposed 12m road width, Dhankhola - Bhalubang Section, Existing avg. road width - 6m





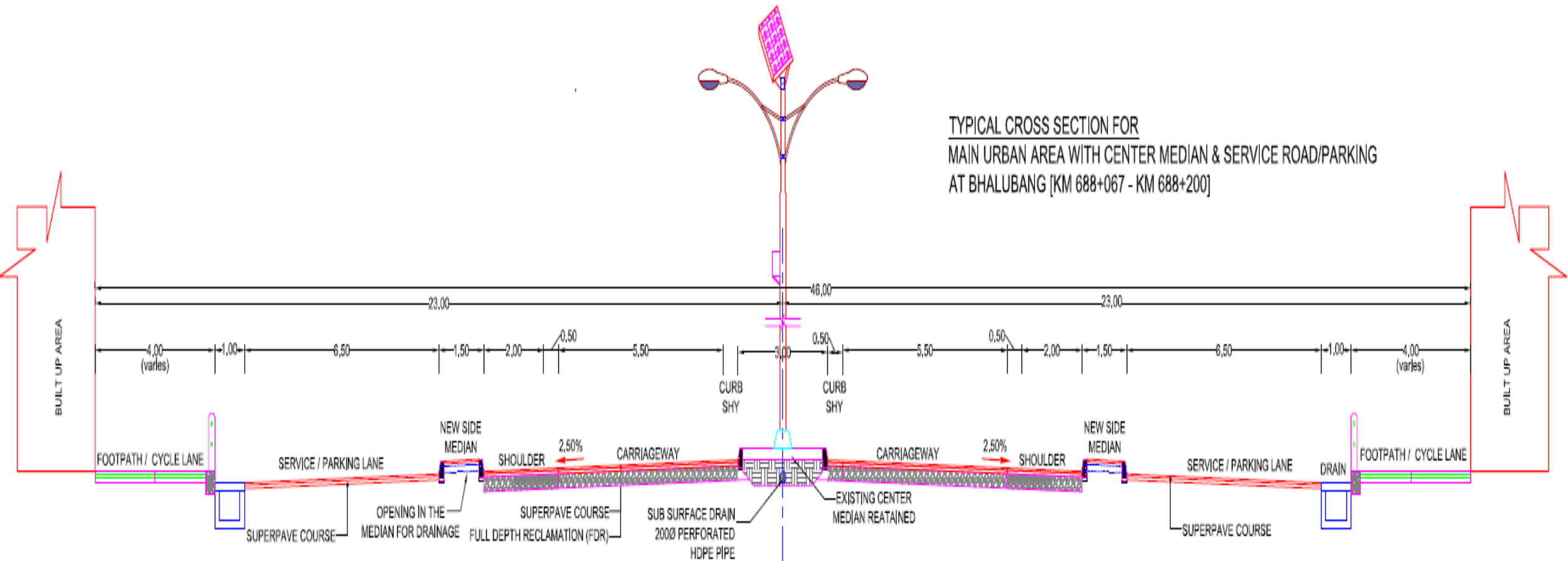
Proposed 12m road width, Bhalubang - Lamahi Section, Existing avg. road width - 6m



11/30/23
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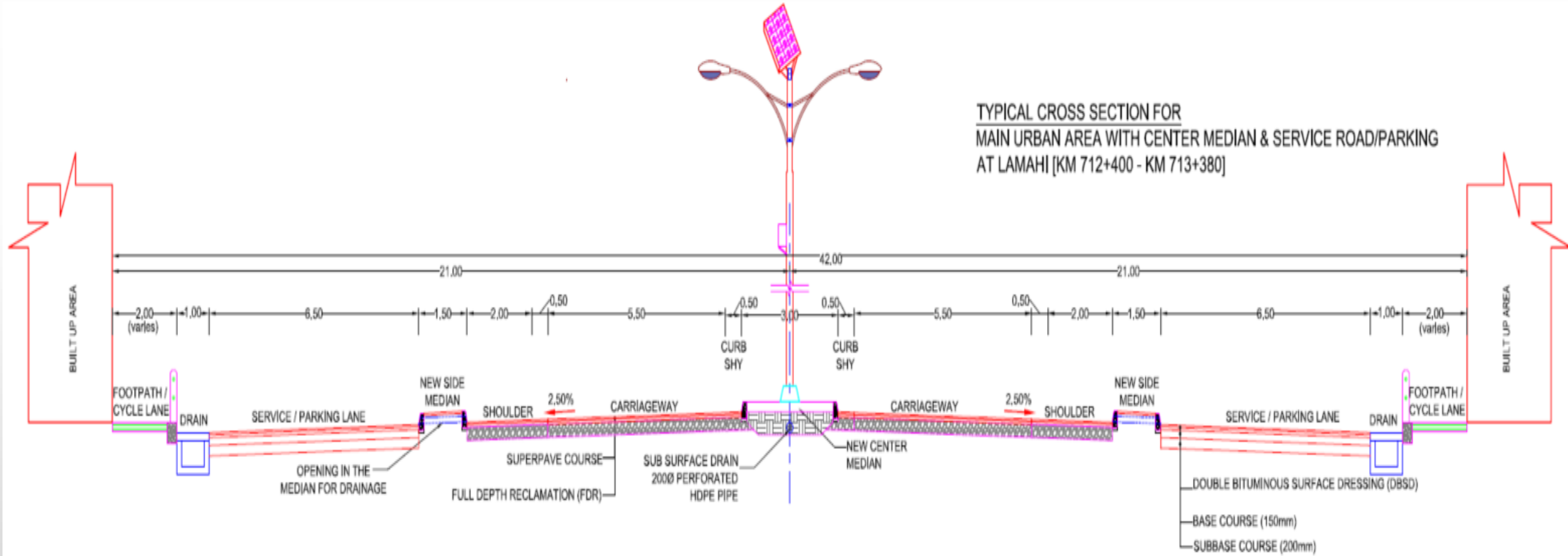
Road Design Details



Typical Section: Plain Major Urban Area at Bhalubang



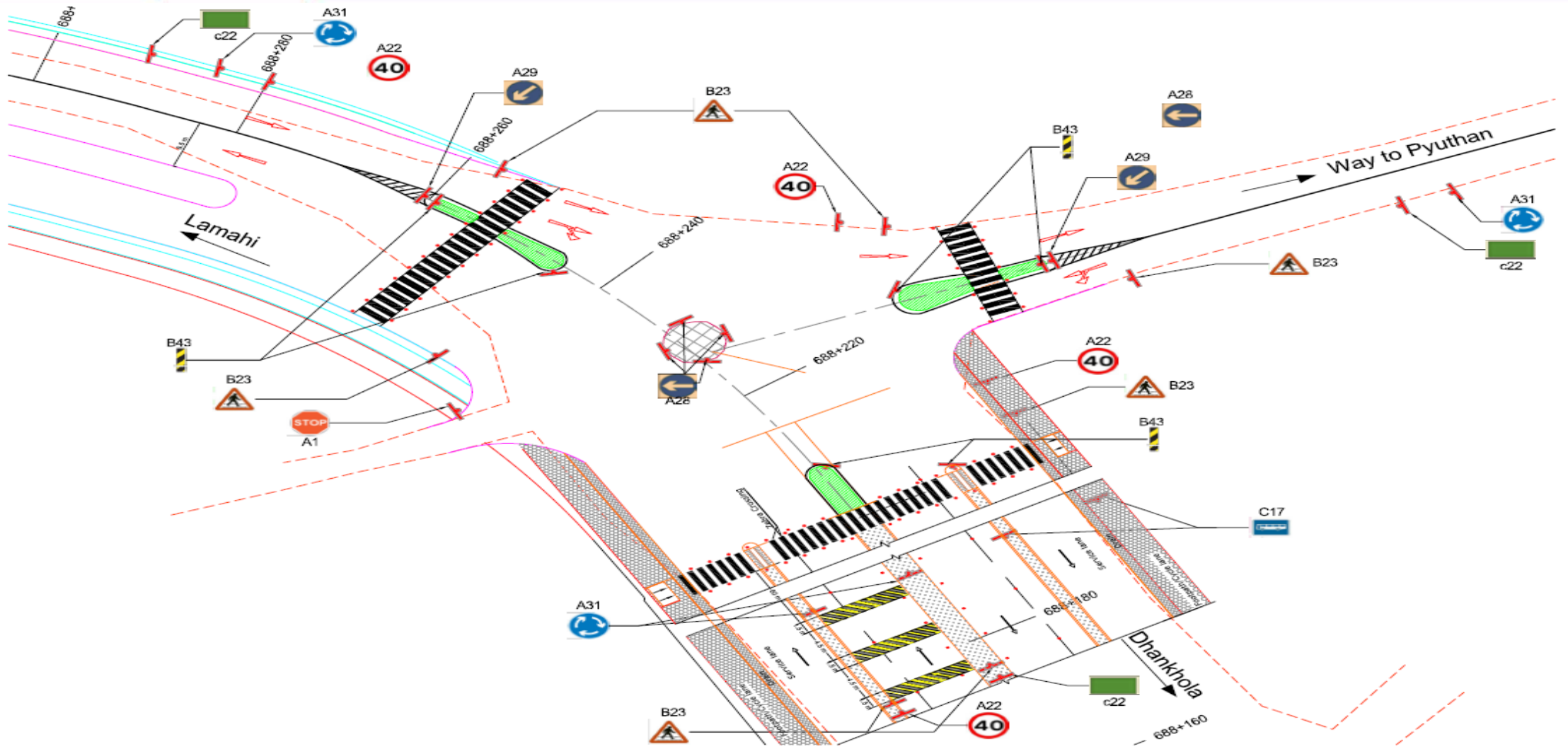
Road Design Details



Typical Section: Plain Major Urban Area at Lamahi



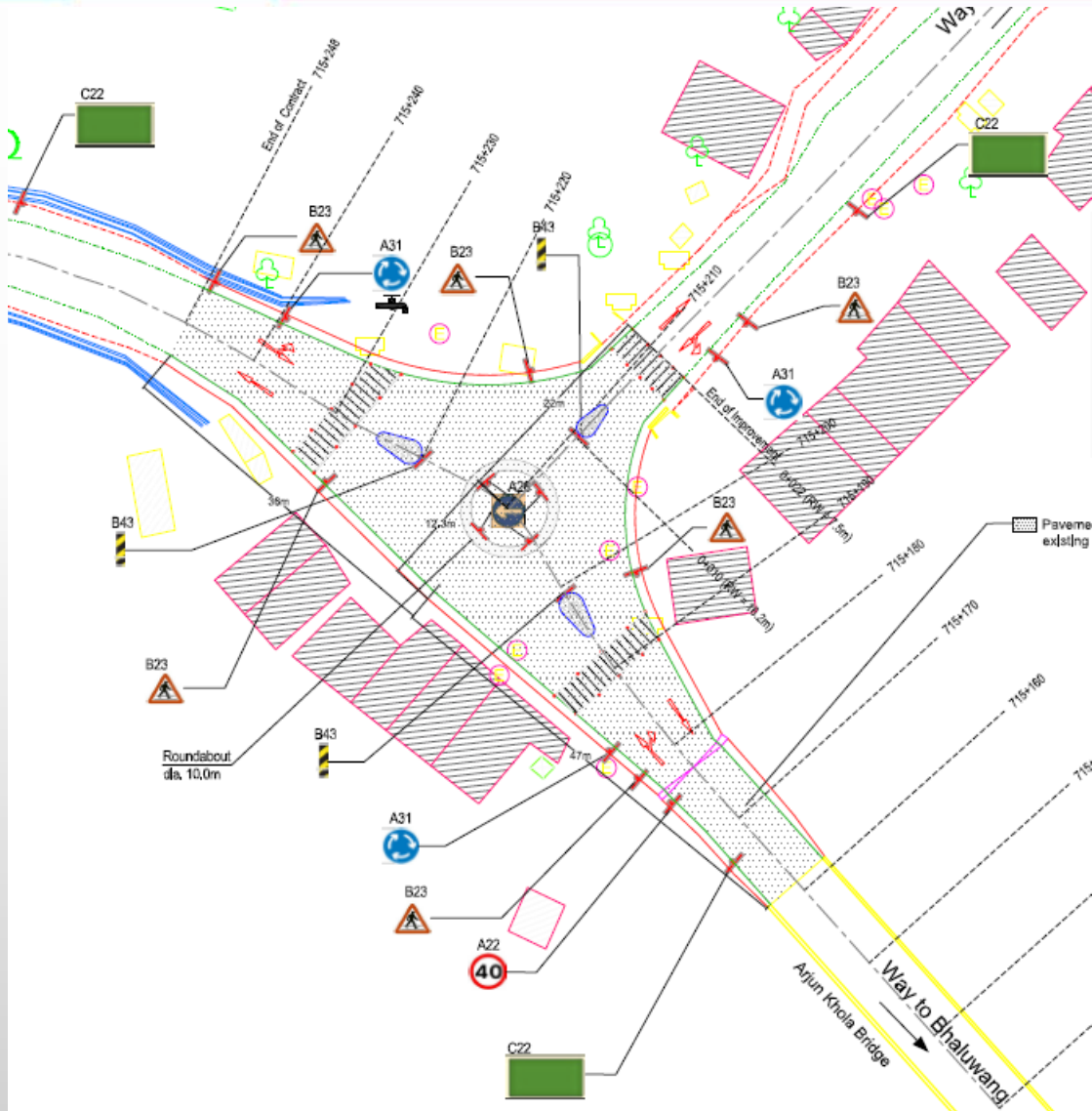
Road Design Details



Junction Improvement at Bhalubang



Road Design Details



Junction Improvement at Arjun Khola



Road Design Details



Retained	Retained and Extended. Extension usually 2 m at each side of the existing structure	To be Replaced with RCBC due to hydraulic or structural reasons	Total
6	76	6	88

Details of Slab Culvert Improvement

Retained	Retained and Extended. Extension usually 2.50 m at each side of the existing structure	To be Replaced due to hydraulic or structural reasons.		Other: Add new, rebuild, remove	Total
		PC	BC		
6	28	74	10	4	122

Details of Pipe Culvert Improvement

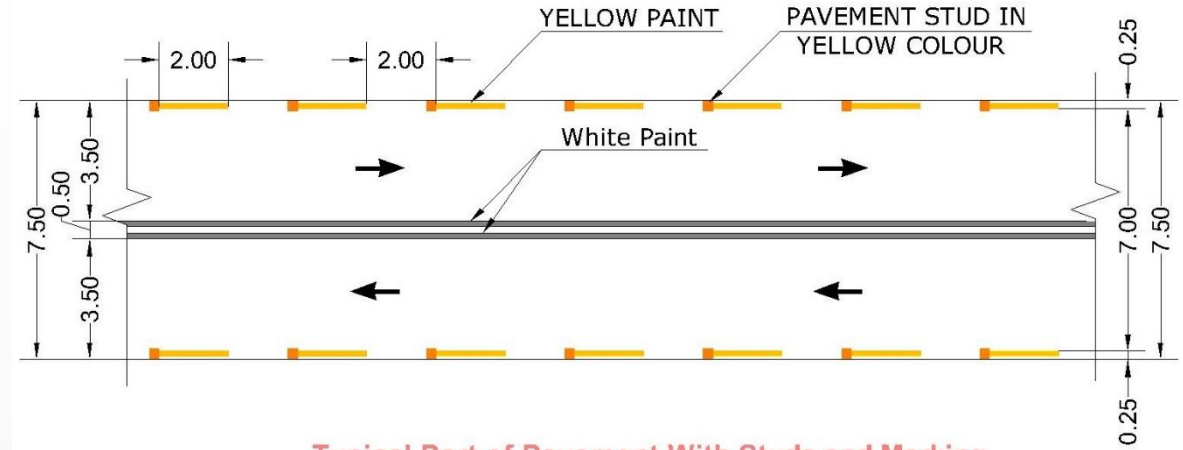
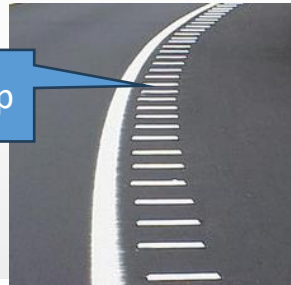


Road Design Details

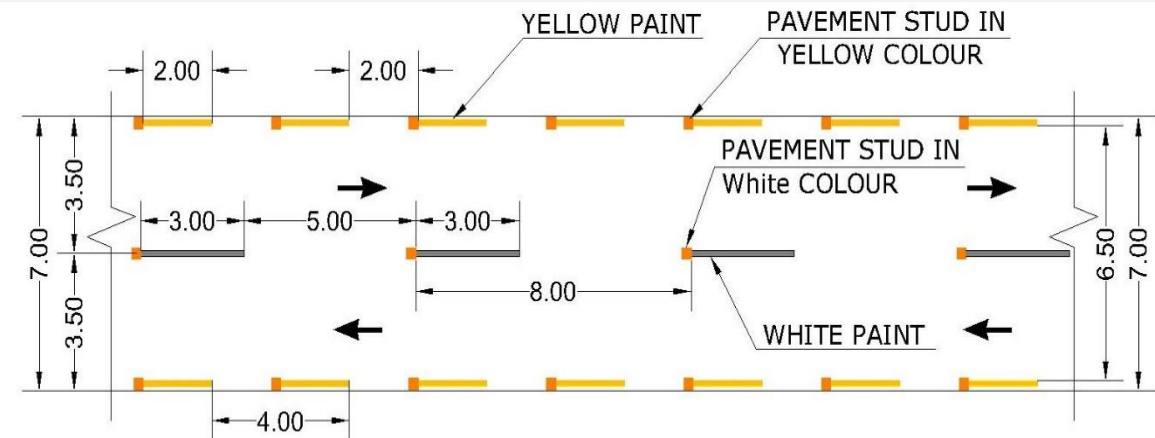
Traffic Safety

- Provision of road marking and traffic signs as per DoR Traffic Signs Manual Vol. 1 & 2
- Provision of painted rumble strip at pedestrian crossings at settlement areas
- Provision of crash barriers at sharp curves along with Chevron.
- Road studs
- Safety element at bridges

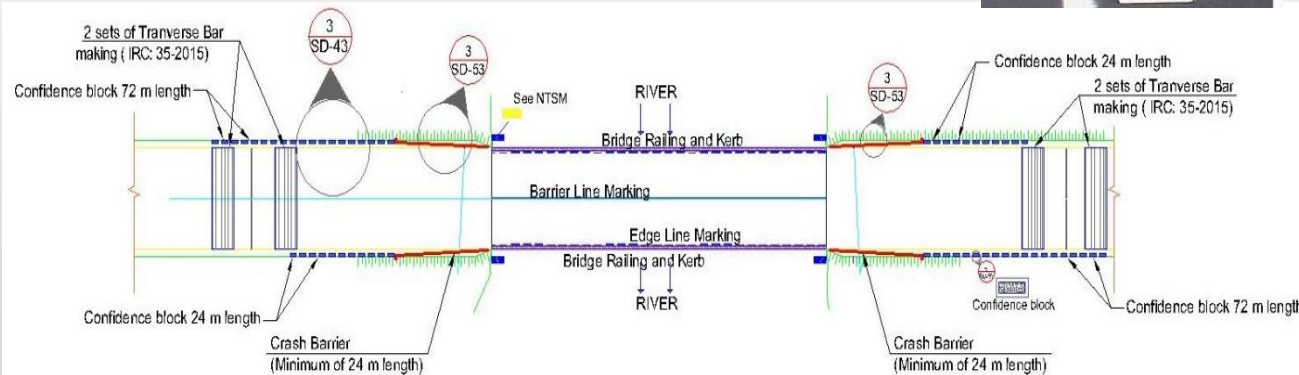
Painted rumble strip



Typical Part of Pavement With Studs and Marking
Dhankhola to Bhaluwang



Typical Part of Pavement With Studs and Marking
Bhaluwang to Lamahi



Schematic Diagram of Safety Treatment at Approach to Existing Bridges



Road Design Details

- **Repair and Maintenance of Bridges and Slab Culverts**

- No widening to 24 nos. of bridges (Avg. width is 7.0m)
- Repair and Maintenance of bridge's bearings and other elements such as railings, deck slabs, curb stones, painting, drainage spouts, expansion joints, crack sealing and provision of minor river training works
- Safety features at the approach to the bridges



Construction & Implementation Schedule



Construction Timeline

- Tendering and Procurement: 4 months
- IFB Issuance: 30 October 2024
- Contracting planned: March 2024
- Construction Period: 24 months
- Project Monitoring and Evaluation: Defects Notification Period (DNP) 12 months, 3.5 months Post DNP

Procurement	Construction Period	DNP + Post DNP
4 months	24 months	12 months + 3.5 Months



Roads and Electricity, Way to Prosperity

Annex D

Questions and Answers

#	Questions	Answers
Questions sent via Zoom message:		
1.	Is there any restriction on participation of government owned entity from foreign country in this tender?	<p>Please refer to the Bidding Document clause ITO 5.6 which states “Government-Owned Enterprises (“GOEs”) are not eligible to compete for MCC-funded contracts for goods (which includes contracts for the supply and installation of information systems) or works. GOEs (a) may not be party to any MCC-funded contract for goods or works procured through an open solicitation process, limited bidding, direct contracting, or sole source selection; and (b) may not be prequalified or shortlisted for any MCC-funded contract for goods or works anticipated to be procured through these means. This prohibition does not apply to Government-owned Force Account units owned by the Government of the Accountable Entity's country, or Government-owned educational institutions and research centers, any statistical, mapping or other technical entities not formed primarily for a commercial or business purpose, or where a waiver is granted by MCC in accordance with the MCC PPG. All Offerors must certify their status as part of their Offer submission”</p> <p>You are also requested to refer to the Accountable Entity Procurement Policy & Guidelines (PPG), PART 8. GOVERNMENT-OWNED ENTERPRISES, which states “GOEs are not eligible to compete for MCC-funded contracts for goods or works.” For complete details, please go through Part 8 in detail.</p>
2.	Can we get a copy of pre-feasibility project IEE (Initial Environment Examination) Report for this project Does the contractor need to obtain permission from local bodies for environmental health, and safety / apart from the government authorities	Yes. The IEE report is available for the Contractors. MCA-Nepal will obtain the required permits including forest clearance permit. The Contractor should coordinate with the Engineer and the MCA-Nepal on time, if any new permits are required.
Questions raised in the floor		
3.	What is your aspect of Relocation of NEA electricity poles regarding Contractor's scope of work? This is a serious/crucial matter, because difficulty in shifting electricity poles/transformers will affect the whole construction project. Let's put this in the MCA-Nepal scope not in the Contractor scope.	<p>The relocation of electricity poles utility will be done by the Contractor in coordination with MCA-Nepal and Nepal Electricity Authority (NEA). The Contractor has to engage NEA personnel for utility relocation by obtaining NEA rates. MCA-Nepal has already made a request for site approval from NEA which will be provided by NEA before the Contractor is on-board.</p> <p>MCA-Nepal cannot directly engage NEA for electric poles relocation because both are GoN agencies and would be against MCC PPG. Engaging a separate contractor for only electric poles relocation would create conflict in communication and coordination with the main contractor that is why we have kept this item in the main FDR contractor’s scope of work and it will not be changed.</p>

#	Questions	Answers
4.	As this is a time constraint project. If delay/obstruction happens mainly due to the relocation of electricity poles, what will be the consequences for the Contractor.	Any delay will be dealt as per the provisions of the Contract by analysing each and every delays.
5.	We do not see any limitation in the number of JV partners for a JV submission in the bidding document. Can you please clarify?	There is no limitation in the bidding document for the number of JV partners for a JV submission.
6.	Is BoQ item 2.06 related to relocation items, Lump Sum or else?	It is Lump Sum.
7.	In the experience of Key activities, is the share value of work done applicable or not for the JV partners in a JV submission?	<p>In case the Offeror has carried out key activities in a Joint Venture with someone else, the experience of the Offeror will be equal to their share of work in their Joint Venture Agreement. Only the share of key activities carried out by the Offeror shall be stated in the Offer and that shall be considered.</p> <p>As per the requirements of section III, Qualification and Evaluation criteria, the Lead Member (in case of Joint Venture) or the Offeror who submits an Offer (in the case of a Single Entity) must meet the requirement. However, for the Specific Experience in Key Activities above, the requirements may be met by a proposed specialized sub-contractor(s). However, if the Offeror proposes more than one sub-contractor, the experience of the sub-contractors will be added for key activities.</p>
8.	Who with what positions (experts) will be available at site, and at their Kathmandu office as a Supervision Engineer?	MCA-Nepal appointed representatives (Engineer and MCA-Nepal staffs) will be available.
9.	Can you please elaborate the ITO Clause 19 (d) a bit more? What type of reasons do the clause require to replace Key Personnel. This is a practical problem for bidders in case the bidding process is delayed, and the Personnel proposed during offer submission may not be available for a long time.	If the Offeror fails to provide a replacement Key Personnel with equal or better qualifications, or if the provided reasons for the replacement or justification are unacceptable to the Employer, such Offer may be rejected. The provided reasons for the replacement should be justifiable such as death, medical condition or retirement of the person.
10.	What happens when the reference check for the contractor's past performance check is not received by MCA-Nepal?	We will write to the bidder requesting to follow-up with their referees and we may request alternate references, if the referees are not responding.
11.	How will you define the Lead member of a joint venture?	The Offeror is required to identify the lead partner in their Offer.
12.	As per the Evaluation criteria in the Bidding document, most bidders in Nepal have Similar experience but they lack experience in Key activities.	As per the requirement of section III, Qualification and Evaluation criteria, for the Specific Experience in Key Activities above, the requirements may be met by the proposed specialized sub-contractor(s).
13.	Can you please elaborate a bit on ITO Clause 25? Is there any option of withdrawal or modification in the drop box?	25.1 An Offeror may withdraw, substitute, or modify its Offer after it has been submitted, but before the deadline for submission of Offers, by sending a written notice - through the File Request Link indicated in ITO Sub-Clause 22.1 c) - duly signed by an authorized representative, and shall include a copy of the authorization in accordance with

#	Questions	Answers
		<p>ITO Sub-Clause 21.3 (except that withdrawal notices do not require copies). The corresponding substitution or modification of the Offer must accompany the respective written notice. All notices must be: a) prepared and submitted in accordance with ITO Clause 21 and ITO Clause 22 and, in addition, the respective submissions shall be clearly marked “Withdrawal,” “Substitution,” “Modification;” and b) received by the Employer prior to the deadline prescribed for submission of Offers, in accordance with ITO Clause 23. 25.2 Offers requested to be withdrawn in accordance with ITO Sub Clause 25.1 shall not be opened. 25.3 No Offer may be withdrawn, substituted, or modified in the interval between the deadline for submission of Offers and the expiration of the period of Offer validity specified by the Offeror on the Letter of Offer or any extension thereof.</p>
14.	<p>If we need to modify only some section(s) of our Bid/Offer, how will that be done?</p>	<p>The original submission will be replaced by the modified section. However, modification of paragraphs and/or small sections are not allowed, as this will create issues during the substitution. Thus, if the Offeror wants to submit a modification to the original submission, they must modify the complete document or complete volume. Otherwise, MCA-Nepal shall not be responsible for any error with respect to the evaluation of a wrong file.</p>
15.	<p>Can you please elaborate the ITO/BDS Clause 34.2(b) where the Employer may require up to 15% of Performance Security? How will you determine this?</p>	<p>After the evaluation of the information and detailed price analyses presented by the Offeror, if the offer is found to be significantly unbalanced or front loaded, the Employer may, as appropriate: (a) accept the Offer; or (b) require that the total amount of the Performance Security be increased at the expense of the Offeror to a level not exceeding the percentage specified in the DS; or (c) reject the Offer.</p> <p>For complete details please read ITO 34.</p>
16.	<p>Can a FDR Sub-contractor for an offeror be a sub-contractor for other participants (offers) also?</p>	<p>Please note that the Conflict of Interest clause ITO 5.8 (d) of the Bidding document states: “participate in more than one Offer in this process; participation by an Offeror or any party constituting the Offeror in more than one Offer will result in the disqualification of all Offers in which the party is involved; however, this provision does not limit the inclusion of the same subcontractor in more than one Offer”.</p>
17.	<p>Can the requirement of FDR quantity of 81,600 Cum in the Key activities be fulfilled with the experience having FDR quantity in unit "meter"?</p>	<p>No. The quantity should be in Cum for evaluating against the stated requirement.</p>
18.	<p>Section III of the Bidding Document Vol-1, Clause 14 Specific Experience in Key Activities, "Must meet" is written in the Lead Member column. Whereas in the note below it is mentioned that "<i>For the Specific Experience in Key Activities above, the requirements may be met by the proposed specialized sub-contractor (s). For calculation of all members combined experience, the experience of specialized sub-contractor(s) can also be</i></p>	<p>Please be clarified that currently the provision is that the Offeror can satisfy the requirement through the lead partner or through a sub-contractor, until and unless there is any amendment to this requirement.</p>

#	Questions	Answers
	<i>considered</i> " is mentioned in the note. Are they not conflicting with each other?	
19.	If a prime contractor had given the 100% FDR to a sub-contractor, whose experience would be counted?	The experience will count for the entity holding the experience certificate. The prime contractor will have experience in getting this work done under the Contract. The prime contractor can issue an experience certificate to the subcontractor to validate their role.
20.	Practice average of best 3 years T.O. are taken. But here it is written as last 3 years	As per the bidding document, the turn-over from the most recent three consecutive years will be evaluated.
21.	Can the domestic bidders be the lead partner in a joint venture based on the FDR experience requirement mentioned in the Bidding document? How?	There is no such constraint stated in the Bidding Document. Regarding FDR requirement stated under factor 14, the requirement can be fulfilled by a specialized Sub-Contractor.
22.	What is the meaning of BIR in page-25 of the Bidding document volume 1, in footnote 3 ?	BIR refers to the "Business Information Report".
23.	What kind of document is required for a specific subcontractor? Normally one agreement and their Experience certificate are required as per our past experience/practice. Do we need to submit their turnover and other data? Will their Financial document be necessary to submit?	For a specific subcontractor, submitting the agreement between the contractor and subcontractor and their specific experience, plus registration certificate is sufficient. Financial documents, such as their turnover or other financial data, are not required.
24.	Is the previous project's 50% share which is less than the current requirement of 25% (minimum requirement as per this bidding document) be eligible to pass?	The question is not clear. However, any percentage which is not in compliance with the requirements is not acceptable and the proposal might be rejected for the non-compliance.
25.	Are we supposed to follow the NRB index for previous experience? Will the time value of money be counted for older projects in terms of the Contract amount executed?	The NRB (Nepal Rastra Bank) exchange rate at the time of contract signature should be used to convert the contract amount from NPR to USD. No escalation in amount is permitted.
26.	For Drop box submission, you suggested making a folder rather than files to upload. Are the technical and financial offers supposed to be in the same or different folders?	The technical and financial offers need to be submitted together in the same folder.